

P. SITE LIGHTING**Design Regulations:**

DR.P-1: The following areas shall be illuminated at night to insure the safety of users and to minimize opportunities for crime.

- Surface parking lots
- Parking structures, including access points elevators, and stairwells
- Pedestrian walkways and paths
- Plazas
- Pedestrian Paseos
- Sidewalks
- Automated Teller Machines (ATMs)
- All entrances to mixed-use, cottage commercial, and civic buildings, including rear and service entrances
- All common entrances to multi-family residential units and multiples units
- Garbage disposal areas
- Alleys
- Other areas that are routinely used by pedestrians

DR.P-2: Site, building, and sign lighting shall be located and directed to light the intended area of illumination and to prevent off-site glare impacts on adjacent buildings or properties.

Design Guidelines:

DG.P-1: Lighting should be provided at regular intervals to prevent the creation of light and dark pockets. Dark pockets can create uncomfortable areas for pedestrians and provide opportunities for criminals to hide in dark shadows. Light pockets can create a “fish bowl” affect. Within the light pocket (or the “fish bowl”), pedestrians may be observed, but their ability to see outside of the light pocket is limited, which creates discomfort and insecurity.

DG.P-2: Over-lighting of buildings and sites should be avoided. Over-lighting can create an environment that feels like a prison-yard and can ruin desired night-time ambience.

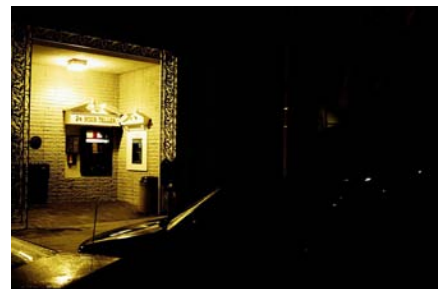
DG.P-3: All lighting poles and fixtures should have a decorative/ornamental design that complements the structures on the site.

DG.P-4: Lighting within storefront windows is encouraged to illuminate the sidewalk and create a desirable night-time ambience.

Examples of safe and desirable night-time lighting:



Examples of poor and undesirable night-time lighting that creates dark and light pockets:



Q. PLAZAS/PEDESTRIAN PASEOS**Design Regulations:**

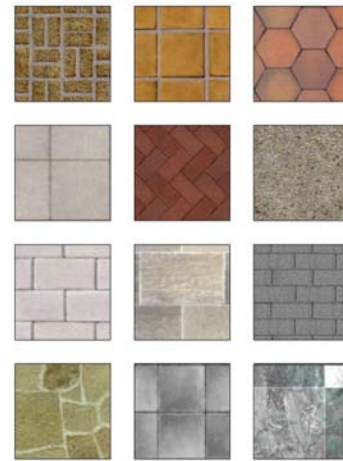
DR.Q-1: All plazas and pedestrian paseos shall be designed with an ornamental surface that is differentiated from the concrete sidewalk and asphalt streets and parking lots. Appropriate types of ornamental paving include:

- Granite tile or stone
- Turf block
- Brick
- Concrete unit pavers
- Concrete with special textures, colors, and patterns

DR.Q-2: At least two sides of a plaza shall be defined by building facades with active ground floor uses (such as restaurants, retail stores, cafes, bars, etc.). Edges that are not defined by building facades shall be defined with landscaping features, such as trees, low planters, seating, a pergola with vines, or sculptures.

DR.Q-3: All plazas shall be designed with pedestrian amenities, such as seating, outdoor dining tables with umbrellas, planters, trees, vine covered pergolas, drinking fountains, pedestrian scaled lighting, public artwork, and fountains. All amenities shall be consistent with approved Downtown Rocklin Streetscape Furniture Palette (refer to Appendix B).

DR.Q-4: Plazas shall be illuminated from dusk to dawn. A combination of overhead lighting, lighted bollards, and uplighting of vertical elements (trees, fountains, sculptures, buildings, walls, etc.) shall be used.



Examples of appropriate paving surfaces and patterns for public plazas



Example of a public plaza that is defined by building facades with active ground floor uses on two sides of the plaza



Examples of a public plaza and pedestrian paseo that is improved with pedestrian amenities

Design Guidelines

DG.Q-1: Fountains and/or water features should be incorporated into the design of plazas.

DG.Q-2: Mist sprayers are encouraged to cool public spaces during the warm summer months.

DG.Q-3: A combination of hard and soft surfaces shall be incorporated into the design of the plaza to add visual interest and variety.

DG.Q-4: Granite rock and railroad and quarry artifacts should be incorporated into the design of plazas and pedestrian paseos to highlight Rocklin's unique history and heritage.

R. PARKS

Design Regulations:

DR.R-1: A master plan shall be prepared and approved for all parks within Downtown Rocklin (Heritage Park and Quarry Park).

DR.R-2: To the extent feasible, parks shall be designed to preserve mature oak trees, natural topographic features, rock outcroppings, and quarry pits and quarry ponds.

DR.R-3: All parks shall be designed with pedestrian amenities, such as shaded trails and paths, seating areas, picnic tables, barbeque areas, planters, trees, vine-covered pergolas, drinking fountains, pedestrian scaled lighting, public artwork, and fountains. All amenities shall be consistent with approved Downtown Rocklin Streetscape Furniture Palette (refer to Appendix B).

DR.R-4: Parks shall be visible from streets, sidewalks, and adjacent uses to facilitate informal surveillance of the park and to increase safety and security. Edge treatments, such as landscaping and fencing shall not block public views into the park. Parks shall not be isolated or walled off from the surrounding community.

DR.R-5: Lighting shall be provided for pedestrian paths, parking lots, restrooms, picnic areas, gazebos, and other structures within parks. Lighting shall be located and directed to control off-site glare.



Example of a quarry pond incorporated into a park setting

DR.R-6: Parks shall be designed with a number of shaded areas to create cool areas during warm summer months. Canopy trees, trellises, gazebos, and/or other structures shall be provided to shade pedestrian paths, picnic areas, outdoor seating areas, and playgrounds.

Design Guidelines:

DG.R-1: Parks should be designed with an attractive path and trail system that provides convenient access to and from the internal features of the park (picnic areas, playgrounds, sitting areas, grass areas, etc.,).

DG.R-2: With the exception of rock climbing and rappelling, Downtown parks should be designed for informal or passive recreation. Grass areas that can accommodate a range of informal recreational activities should be provided rather than sport fields and ball courts.

DG.R-3: Playground equipment and interactive play features should have a theme that represents an element of Rocklin's History.

DG.R-4: When possible, paths should be aligned with important viewpoints, such as mature trees, fountains, or a statue, to create a vista within the park.

DG.R-5: Grass, landscaping, and permeable surfaces should be used within parks to the maximum extent feasible. The use of impervious surfaces, such as concrete and asphalt, should be limited to paths, parking lots, and sports courts (if provided).

DG.R-6: Public restrooms should be provided in each park. Restrooms should be conveniently located near heavily-used areas and should be visible from public streets, sidewalks, pedestrian paths, and adjacent land uses to provide informal surveillance of the facility.

DG.R-7: Granite rock and railroad and quarry artifacts should be incorporated into the design of parks to highlight Rocklin's unique history and heritage.



S. PARKING**Design Regulations:**

DR.S-1: For mixed-use development projects with a shared parking solution, the minimum required number of on-site parking spaces shall be determined based on the following standards:

- Retail, Personal Service, and Eating/Drinking uses as listed in Table 1 of this Regulating Code: 4.5 spaces per 1,000 square feet of floor space
- Professional Office uses as listed in Table 1 of this Regulating Code: 3 spaces per 1,000 square feet of floor space
- Attached studio and 1-bedroom residential units: 1.0 spaces per unit
- Attached residential units with 2 or more bedrooms: 1.5 spaces per unit
- Detached single-family residential units: 2 spaces per unit

DR.S-2: The number of parking spaces required for Medical Office, Public/Civic, Entertainment, and Automobile Sales/Service uses (as listed in Table 1 of this Regulating Code) shall be determined based on either the requirements in Section 17.66 (Off-Street Parking) of the Rocklin Zoning Ordinance or based on a project-specific parking solution that is approved by the Planning Commission. The parking solution shall be based on a parking analysis submitted by the project applicant.

DR.S-3: Parking requirements may be reduced by up to 50 percent with a parking solution approved by the Planning Commission. Potential ways to reduce parking include shared parking solutions, the payment of in-lieu parking fees for public parking lots or structures, or the use of valet parking. Applicants must submit a parking analysis to justify the proposed reduction in parking.

DR.S-4: An adequate number of handicapped parking spaces shall be provided to meet the requirements of the California Code of Regulations, Title 24.

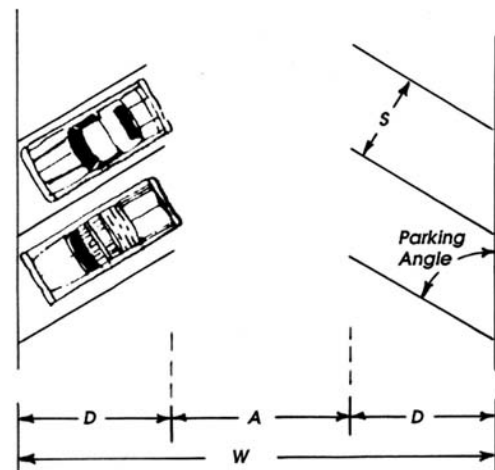
DR. S-5: On-street parallel parking spaces shall have a dimension of 8 feet by 20 feet (for non ADA accessible spaces).

DR.S-6: Parking stalls may be located directly off the alley provided that the alley is expanded to an appropriate aisle width.

DR.S-7: All parking spaces within a surface parking lot and within a parking structure shall be striped.

DR.S-8: The dimensions of off-street parking spaces shall be based on the following standards:

PARKING LOT DIMENSIONS:				
One-Way Drive Aisles				
Parking Angle	W	D	A	S
90	60'	18'	24'	9'
75	56'	19' 1"	17' 10"	9'
60	52' 6"	19'	14' 6"	9'
45	48'	17' 8"	12' 8"	9'
Two-Way Drive Aisles				
Parking Angle	W	D	A	S
90	60'	18'	24'	9'
75	60'	19' 1"	21' 10"	9'
60	58'	19'	20'	9'
45	55' 4"	17' 8"	20'	9'



T. STREETS**Design Regulations:**

DR.T-1: A curb radius between 5 feet and 15 feet shall be provided at all street intersections. A tight turning radius is required to minimize the length of pedestrian crossings and to discourage vehicles from turning corners at high vehicle speeds. To allow adequate space for emergency vehicles (fire trucks), a 25-foot radius clear zone shall be established free of all vertical obstruction, including but not limited to telephone poles, sign poles, fire hydrants, electrical boxes, benches, or newspaper stands.

DR.T-2: All streetlight fixtures, traffic signals, traffic and directional signs, pedestrian wayfinding signs, parking signs, and parking meters, and fire hydrants shall be located within one to three feet of the curb face.

DR.T-3: If provided, street trees shall be planted within three feet of the curb face and shall be spaced at 30-foot intervals to provide a continuous canopy of shade over the sidewalk.¹ Root guards shall be installed for each street tree to minimize damage to the sidewalk. Metal tree grates are required if the street does not include a parkway (a grass strip located between the sidewalk and the curb).

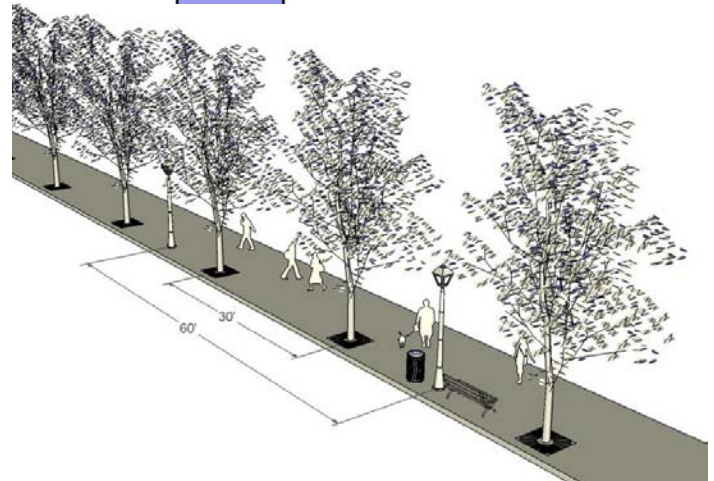
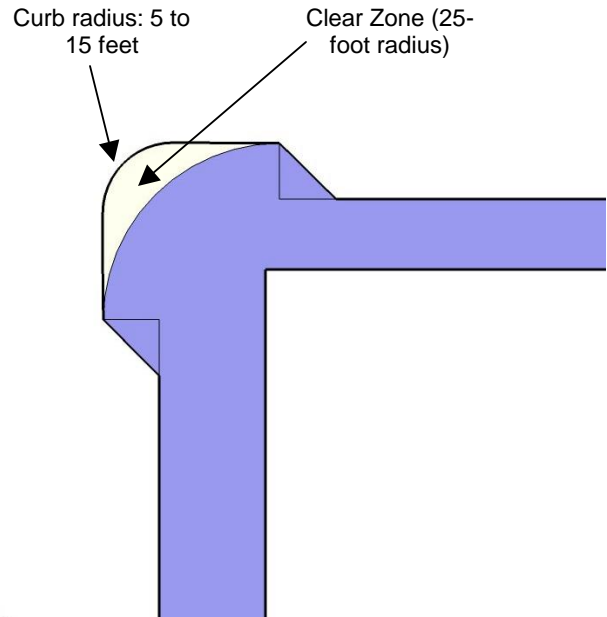
DR.T-4: If provided, streetlights shall be provided at 60-foot intervals or between every two street trees.¹ Streetlights shall be scaled to pedestrians and shall be no taller than 14 feet.

DR.T-5: If provided, streetscape furniture shall be located to maintain a clear pedestrian path of at least five feet in width.

DR.T-6: If permitted, tables, chairs, and other obstructions used for sidewalk dining shall be located to maintain at least six feet of unobstructed sidewalk width.

DR.T-7: All streetscape furniture and amenities shall be consistent with the approved Downtown Rocklin Streetscape Furniture Palette (refer to Appendix B). This Palette shall be used in all Downtown Districts.

¹ The spacing of trees and streetlights may be altered to accommodate existing driveway curb cuts.

**Design Guidelines**

DG.T-1: Existing alleys should be maintained and improved to provide access to parking and service areas behind buildings and to minimize the need for curb cuts along street frontages. Multi-functional alleys that also serve as pedestrian paseos or corridors lined with storefronts are also encouraged.

DG.T-2: New alleys are encouraged on newly created blocks within Downtown Rocklin.

DG.T-3: All streets should be designed to maximize opportunities for on-street parking.

DG.T-4: Existing and new utilities should be placed underground in the right-of-ways of public or private streets and/or alleys.

Design Guidelines for Pacific Street:

Pacific Street will function as the “Main Street” of Downtown Rocklin. It will be designed to encourage pedestrian activity and window-shopping. The street will have two travel lanes in each direction. On-street parking (parallel on one side and angled on the other) will be provided to slow down vehicular traffic and to provide a buffer between vehicles and pedestrians on the sidewalks. The angled and parallel parking will switch sides after each block to serve as an additional traffic-calming feature. The street will have wide sidewalks to accommodate street trees, pedestrian scaled lighting, streetscape furniture, and outdoor dining. Trees will be planted in tree grates to maximize usable sidewalk width for pedestrians. Sidewalk bulb-outs and specially paved crosswalks will be provided at all intersections and at mid-blocks. Intersections will be signal or stop controlled to slow vehicle traffic and to provide opportunities for pedestrians to cross the street safely. Cutouts for bus stops will be provided along the street.

Design Speed: 25 MPH

Travel Lanes: Four Lanes (two in each direction)

Right-of-Way Width: 100 Feet

Curb Type: Raised

Typical Sidewalk Width: 15 Feet

Bicycle Lanes: None

Parkway: None

Landscaping: Street trees in grates should be required and regularly spaced at 30-foot intervals. Trees, shrubs, flowering plants, and granite rock should be required in landscaped bulb-outs

Streetscape Furniture: Benches and trash receptacles should be provided on every block at intervals no greater than 150 feet. Bike racks, drinking fountains, planter boxes, and other streetscape amenities are allowed and encouraged

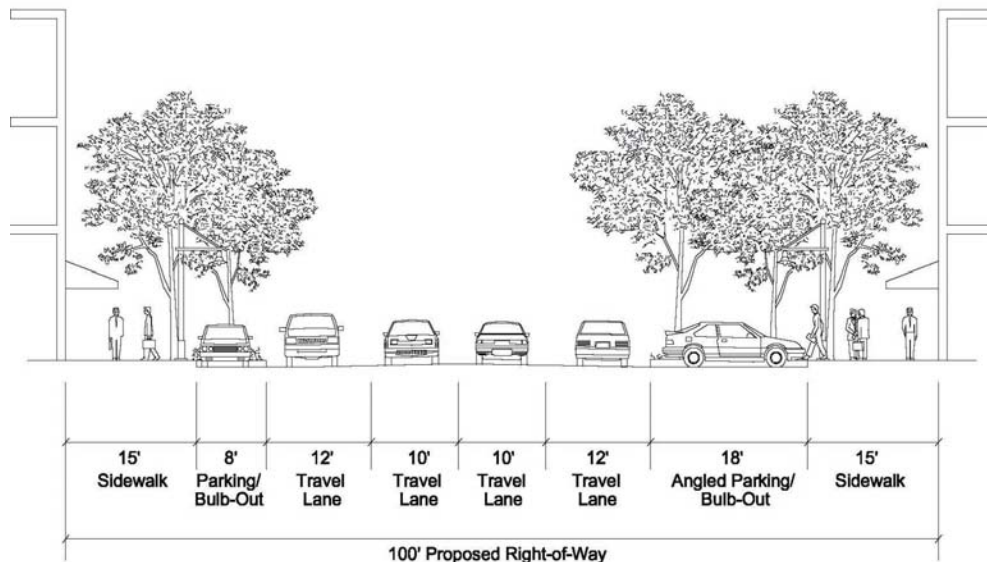
Streetlights: Pedestrian-scaled lighting required

Pedestrian Crossings: Required at all intersections and mid blocks. Pedestrian crossings should be designed with a special pavement that has a differentiated texture and color

Bulb-outs: Required at all pedestrian crossings

Cutouts for Bus Stops: Encouraged between Pine Street and Oak Street and between Civic Center Drive and Farron Street.

Pacific Street



Back-in Angled Parking should also be Considered

**Design Guidelines for Rocklin Road
(Between Railroad Tracks and Meyers
Street):**

Rocklin Road will function as a landscaped boulevard that provides primary access to Downtown Rocklin from Interstate 80. The street will have two travel lanes and a bike lane in each direction. A landscaped central median will divide the travel lanes. On-street parallel parking will be provided to slow down vehicular traffic and to provide a buffer between vehicles and pedestrians on the sidewalks. Sidewalks will have street trees, pedestrian scaled lighting, and streetscape furniture to encourage pedestrian activity. Trees will be planted in tree grates to maximize usable sidewalk width for pedestrians. Sidewalk bulb-outs and specially paved crosswalks will be provided at all intersections to maximize opportunities for pedestrian crossings. Intersections will be signal or stop controlled to slow vehicle traffic and to provide opportunities for pedestrians to cross the street safely. Cutouts for bus stops will be provided near the Civic Center.

Design Speed: 30 MPH**Travel Lanes:** Four Lanes (two in each direction)**Right-of-Way Width:** 100 Feet**Curb Type:** Raised**Typical Sidewalk Width:** 8 to 9 Feet**Bicycle Lanes:** Yes**Parkway:** None

Landscaping: Street trees in grates should be required and regularly spaced at 30-foot intervals. Trees, shrubs, flowering plants, and granite rock should be required in the central median and landscaped bulb-outs

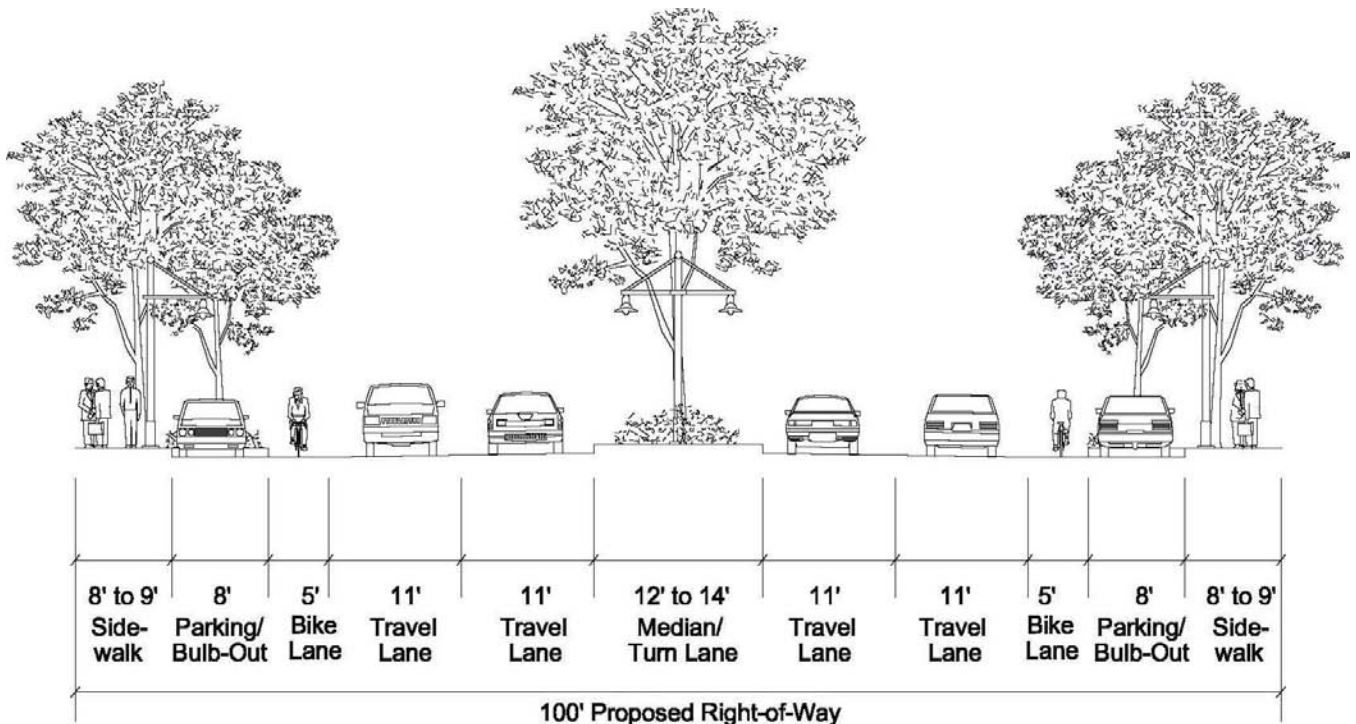
Streetscape Furniture: Benches and trash receptacles should be provided on every block at intervals no greater than 150 feet. Bike racks, drinking fountains, planter boxes, and other streetscape amenities are allowed and encouraged

Streetlights: Pedestrian-scaled lighting required

Pedestrian Crossings: Required at all intersections. Pedestrian crossings should be designed with a special pavement that has a differentiated texture and color

Bulb-outs: Required at all pedestrian crossings

Cutouts for Bus Stops: Encouraged near the Civic Center.

Rocklin Road

Design Guidelines for Railroad Avenue:

Railroad Avenue will function as a local street that provides an alternative north-south route to Pacific Street. The street will have two travel lanes (one in each direction). On-street parallel parking will be provided on the east side of the street to slow down vehicular traffic and to provide a buffer between vehicles and pedestrians on the sidewalks. A pedestrian and bike trail will be provided on the west side of the street. Sidewalks will have street trees, pedestrian scaled lighting, and streetscape furniture to encourage pedestrian activity. Trees will be planted in tree grates to maximize usable sidewalk width for pedestrians. Sidewalk bulb-outs and specially paved crosswalks will be provided at all intersections to maximize opportunities for pedestrian crossings. Intersections will be signal or stop controlled to slow vehicle traffic and to provide opportunities for pedestrians to cross the street safely.

Design Speed: 25 MPH**Travel Lanes:** Two lanes (one in each direction)**Right-of-Way Width:** 50 Feet**Curb Type:** Raised**Typical Sidewalk Width:** 10 Feet**Bicycle Trail:** Yes**Parkway:** None

Landscaping: Street trees in grates should be required and regularly spaced at 30-foot intervals. Trees, shrubs, flowering plants, and granite rock should be required in landscaped bulb-outs

Streetscape Furniture: Benches and trash receptacles should be provided on every block at intervals no greater than 150 feet. Bike racks, drinking fountains, planter boxes, and other streetscape amenities are allowed and encouraged

Streetlights: Pedestrian-scaled lighting required

Pedestrian Crossings: Required at all intersections. Special pavement that has a differentiated texture and color is encouraged for pedestrian crossings

Bulb-outs: Required at all pedestrian crossings

Railroad Avenue

